

**LIGHT DUTY CROSSINGS FOR
DOMESTIC PREMISES**

**VEHICLE AND PEDESTRIAN ACCESS CROSSINGS TO
MAINTAINABLE HIGHWAYS - LIGHT DUTY FLEXIBLE CONSTRUCTION
FOR DOMESTIC PREMISES ONLY**

SPECIFICATION SCHEDULE

<i>Item No.</i>	<i>Description of Work</i>
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KERBS

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| 1 | The junction with the carriageway shall be delineated with a minimum of 3 number 915mm long x 125mm x 150mm kerbs on a 150mm x 380mm base of ST3 (15N/mm ²) concrete (approx 1 cement: 2 sand: 4 aggregate by volume mix). A 125mm x 150mm taper kerb is required on each side of the junction except where there are no existing kerbs adjacent. The Highway boundary at the rear shall also be delineated with 50mm x 150mm square cut concrete edging on a 75mm x 330mm base of ST3 concrete. Where there is no existing footway or the adjacent footway is constructed with a non flexible material (eg slabs or concrete), the edges of any pedestrian or vehicular access shall also be delineated with 50mm x 150mm square cut concrete edgings. |
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CONSTRUCTION

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| 2 | Excavate for as necessary and compact the formation prior to laying any foundation material (all topsoil/vegetation must be removed). In all cases at least 200mm of soil shall be removed prior to placing the sub-base. Provide, lay and compact using vibrating compaction equipment at least 150mm finished thickness of approved non-frost susceptible crushed rock, crushed slag or crushed concrete Type 1 sub base. <i>Exceptionally and with the prior agreement of the Highway Authority the 150mm layer of sub-base may be substituted by a 75mm layer of 20mm size dense binder course recipe mixture to BS4987 using 50 pen or 125 pen binder. Air void content to be within the range 2% to 10%.</i> |
| 3 | Provide, lay and compact 100mm finished thickness of 20mm size dense bitumen binder course to BS4987 using 50 pen or 125 pen bitumen binder. Air void content to be within the range 2% to 10%. |
| 4 | Provide, lay and compact 25mm finished thickness of 6mm size dense bitumen surface course to BS4987 using 125 pen bitumen binder. Air void content to be within the range 2% to 10% |

N.B. All materials described in this schedule shall be from a source currently approved by the Director of Highways and Planning for the purpose. His representatives may at any time sample any of the materials specified and carry out necessary testing, including site testing, to check the conformity of the material and construction methods to the relevant specifications. In the event of any result failing to meet the appropriate specification requirements, the highway authority reserves the right to recover the costs of such tests from the party on whose behalf the access crossing is being installed.

Similarly the right is reserved to amend the thickness and/or materials from those indicated should he consider it necessary.

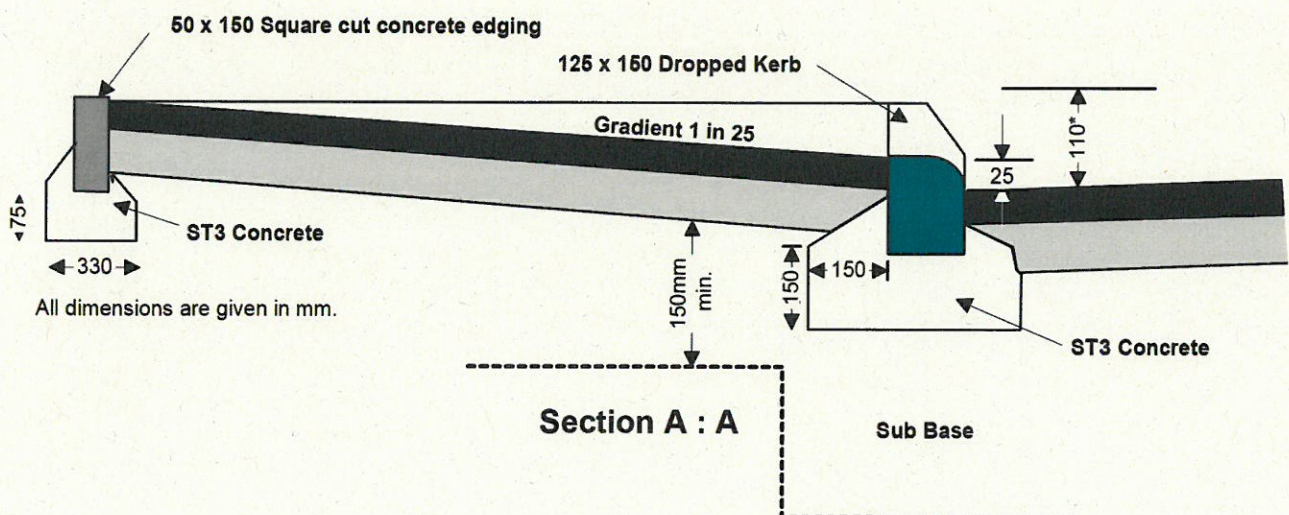
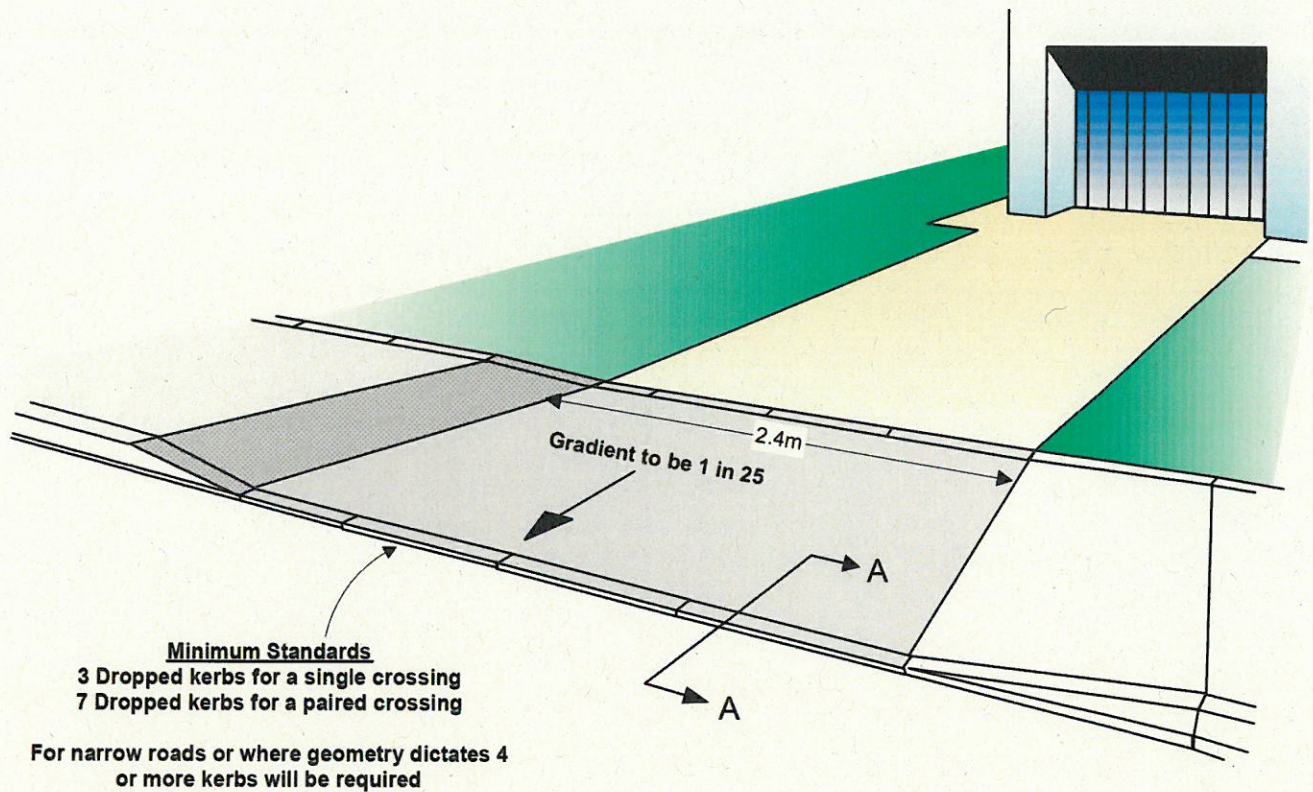
PUBLICATION REFERENCES

BS4987 Coated macadam (asphalt concrete) for roads and other paved areas - Published by British Standards Institution.

Type 1 Aggregate - Lincolnshire County Council Development Road Specification and Construction.

BS5328: Methods for specifying concrete including ready mixed concrete - Published by British Standards Institution.

LIGHT DUTY FLEXIBLE CONSTRUCTION



HIGHWAYS ACT 1980 - SECTION 184
VEHICLE AND PEDESTRIAN ACCESS CROSSINGS TO MAINTAINABLE HIGHWAYS
GENERAL PROCEDURE

Your attention is drawn to Section 184 of the Highways Act, 1980 which relates to the proper construction of vehicle crossings of footways and verges and provides that the design of such crossings must be approved by the appropriate Authority. The appropriate Authority in this instance is the County Council as Highway Authority. Where an occupier of premises habitually permits a vehicle to cross the footway or verge to those premises without properly constructing a crossing, or where it appears to the Highway Authority that a planning permission granted makes it necessary for a crossing, or where it appears to the Highway Authority that a planning permission granted makes it necessary for a crossing to be constructed or a made-up crossing altered, the County Council, after giving due notice, may carry out the work of construction and recover the cost from the occupier/owner of the premises.

When a vehicle crossing has been constructed to my satisfaction, future maintenance will be the responsibility of the County Council.

The construction of vehicle crossings will normally be carried out by the developer with the assistance of a suitable contractor and it will be necessary for him to obtain written permission from the County Highways Department, preferably the Area Manager, before work is started and in order that it may be inspected. In these circumstances, the person undertaking the work must ensure that the requirements of Part III of the New Roads and Street Works Act 1991 are met with regard to undertaker's apparatus. Any person, however, may request the County Council to make up existing crossings or construct new ones. In such cases the payment for work will be in accordance with standard unit rates. Where a developer wishes the County Council to construct the crossing he should communicate with the Area Manager for the area. The further necessary particulars will be obtained by the Area Manager and the developer will be informed of the total cost of the work. The work will be carried out by the County Council as soon as possible after the developer has paid to the County Council the sum quoted by the Area Manager.

Developers will appreciate that where the highway is bounded by a ditch on the frontage of their property, the maintenance of this ditch is the responsibility of the adjacent landowner. In making an access crossing over a ditch an adequate pipe or culvert will be required and where a water course is the responsibility of a drainage authority the dimensions of the culvert and levels must be agreed between the developer and the drainage authority. In other cases of roadside ditches the size of pipe, which will not generally be less than 300mm in diameter, and the line and level at which it is to be laid must be agreed by the Divisional Highways Manager or his Representative and the District Council so that such sections of pipe laid will conform with any future work to pipe the ditch. It will normally be necessary to surround pipes with concrete, particularly if they are less than 1 metre below the access level. Reasonable headwalls will also be necessary. The whole of the work in this category should be carried out by the developer or his contractor and will not normally be undertaken by the County Council, even if requested. The subsequent maintenance of such pipes and culverts remains the responsibility of the developer.

All persons undertaking work within the highway are prohibited from using metal detectors, other than in the course of authorised work.

Any articles of archaeological interest or value found within the highway shall belong to the County Council unless the adjacent landowner has a rightful claim to the article. Any such find shall be reported to the Museums Service with details of the location and depth at which the article was found.

Details of Design

Typical layouts are available for the guidance of developers and the typical section of the access crossing is prepared to show the alternative types of construction and the necessary crossfall. These details may be obtained from the Highways Office as shown below.

Alternative Specifications

Alternative specifications for the construction of pedestrian, domestic and *light industrial vehicle access crossings are set out in the attached specification schedule.

*Light industrial vehicle crossings for the purpose of this document relate to vehicles having a maximum axle load of 4 tonnes. Crossings for vehicles with axle loads in excess of 4 tonnes are classified heavy duty and the construction design in relation to every individual site must be approved by the County Council.

For further information please contact: Lincolnshire County Council, South Kesteven & Sleaford Highways Division
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